



Bill Martin, AICP  
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October 21, 2019

Ninia Hammond  
Integral Communities  
2235 Encinitas Blvd., Ste. 216  
Encinitas, CA 92024

Re: SUB 18-0011 / ENV 18-0009 / PHG 18-0049 – Request for a Specific Plan Amendment, General Plan Amendment (Circulation Element), Master and Precise Development Plan, Tentative Subdivision Map and Development Agreement for Palomar Heights (555 E. Valley Pkwy and adjacent properties)

Dear Ms. Hammond:

The City of Escondido has completed its third review of the above-referenced application, submitted on September 13, 2019. Upon completion of this review it has been determined that there are still outstanding issues that need to be addressed. Please provide the items, information, and plan revisions/modifications described below so that we can complete our review of the application.

**PLANNING DIVISION COMMENTS:**

*Italicized text identifies comments that were provided in a previous comment letter, dated August 2, 2019, and have either not been addressed or require additional. Supplemental information on those items is also included, in **bold** text, if necessary for clarification.*

GENERAL COMMENTS

1. *Provide the Planning Case Numbers (SUB 18-0011 / ENV 18-0009 / PHG 18-0049) and date of revision on all plan sheets. **The case numbers were provided on the Preliminary Grading Plan sheets but were not added to the other plans.***
2. The date of revision shall be provided on all plans and reports.
3. *Provide a fencing plan.*

CIVIL ENGINEERING PLANS

1. *Provide pad and finished floor elevations for the apartment buildings on the Preliminary Grading Plans.*
2. *Identify the name of the soils engineer in the General Design Notes. **This has been addressed on the Tentative Map but not the Preliminary Grading Plan.***
3. *The Existing Alley detail on Sheet 2 shows two 10-foot lanes but a 22-foot total width. **This has been addressed on the Tentative Map sheets but not the Preliminary Grading Plans. However, Engineering comments below may require additional modifications.***
4. No lot numbers are shown on Sheets 4 and 6 of the Tentative Map.

DESIGN CONSIDERATIONS

The overall design of the project is moving in the right direction with regard to the major concerns raised previously. That said, the following design-related issues need to be taken into account when making further project revisions:

Site Planning

1. *The interior of the site is dominated by parking and driveways. **Consider the use of grass-crete (or similar) in various parking space around the project site. This can also help your storm water treatment needs. Vehicle lifts in garages could also provide additional covered***

**parking to make up for removal of surface parking spaces, especially for the three-bedroom apartments.**

2. *Provide porticos or other decorative structures at the end of the private alleyways serving the row-homes and villas. **Decorative structures would help soften the appearance of driveways and garages, from both the public roads surrounding the site and the interior private roads.***
3. Thank you for providing pedestrian access to N. Fig Street. In addition to the switchbacks for the accessible path, is it possible to provide stairs as a short-cut for those do not need the accessible path?
4. *Delivery areas are necessary (for commercial deliveries and residents moving in/out of the apartment buildings). These need to be on the interior of the site. **Your response to this comment states that they are shown on the plans, however they could not be located. Please provide callouts.***
5. *Buildings 1 and 23 need to be redesigned so that there are two ways in/out of the garages (Basement in Building 23, First Floor in Building 1).*
6. *A turn-around area needs to be provided at the end of the garage for Building 18. Same applies to the garage on what is called the First Floor of Building 23. **The turn-around area identified in Building 18 is a parking space. It cannot serve both purposes.***
7. *Identify the location of trash enclosures. If interior to the buildings, callout the location(s) and ensure that they can be accessed for pick-up by the trash hauler. **Trash enclosures have not been called out on the site plan, however they are identified on the building floor plans. The following concerns exist:***
  - a. **There are not enough trash facilities to serve the number of units, particularly in Buildings 1 and 23.**
  - b. **The location of the trash rooms in Buildings 18 and 23 are located too close to the project driveway on E. Grand Avenue and will create traffic/vehicle stacking issues during collection.**
  - c. **A paved surface is necessary between the trash facilities and the location where the trash truck will stage for collection.**
8. An accessible path of travel may not be required on both sides of the internal road network. Removing sidewalks in areas where not required could allow for additional landscaping and open space.
9. Number the first and last parking spaces in each area inside the parking garages. This should be shown on the conceptual site plan sheet as well as the architectural plans for the apartment buildings and senior apartments.
10. Identify locations for bicycle parking, both along the Valley Boulevard frontage and on the project site.
11. On the Conceptual Site Plan, show the sight distance lines or remove the callouts.
12. Provide sections across Private Drive A.
13. The following items shall be addressed on the Preliminary Grading Plan:
  - a. There appear to be existing retaining walls shown in a couple locations on the site. Call them out as existing (or to be removed), and provide top and bottom elevations.
  - b. Reduce the height of retaining walls facing E. Valley Parkway and N. Fig Street so they are five feet or less to provide a more appropriate pedestrian experience. This can be accomplished by terracing walls with landscaped benches between them. The large wall along the northeast property line should be terraced as well. Landscaped benches shall be at least as wide the height of the walls.
  - c. Identify the height of the retaining wall near the corner of Valle Boulevard and E. Grand Avenue.

### Open Space and Landscaping

1. Provide an overall open space exhibit showing the areas classified as open space, and whether they are considered active or passive. (Please note that a currently proposed Downtown Specific Plan amendment being brought before City Council for consideration on October 23, 2019, may revise the open space definition to require any open space area to measure at least 200 square feet in area. Text of the proposed amendment can be found at the following link:  
<https://www.escondido.org/Data/Sites/1/media/agendas/Council/2019/102319CCMeetingAgendaPacket.pdf?v=2.>)
2. The central open space area is not large enough to accommodate a 500-plus unit project and should be expanded and enhanced. Consider the following potential changes to the area:
  - a. Parking spaces adjacent to the open space area could be used for this purpose and the recreation building could be shifted further north.
  - b. Include barbeque areas.
  - c. Provide tiered seating areas on the south side of the area.
  - d. Provide pocket or accordion doors from the recreation building opening onto the pool area.
  - e. The spa may be too close to the adjacent residential units, particularly if the fence between them is not solid.
3. Additional trees should be provided in the following locations:
  - a. Along E. Grand Avenue in front of Buildings 12, 13, and 18.
  - b. In the landscape area next to parking space no. S-35.
  - c. On E. Valley Parkway in front of Building 1.
  - d. At the intersection of private drives A and B, adjacent to Building 23.
  - e. On the north side of Buildings 7 and 8.
4. Include a tot-lot or other "pocket park" in a couple locations throughout the site. Areas that may be good candidates for this type of feature are the parking spaces adjacent to Buildings 4, 5, 20, or 21, and parking spaces on the interior corner of Building 1.
5. Provide seating/benches in the dog park area.
6. A roof deck on the corner of Building 1 would provide a great place for residents to congregate, similar to the type of residential open space amenities previously contemplated with the building re-use scenario for the project.
7. Provide additional enhanced paving in various locations along the interior driveways of the property to soften the appearance of so much asphalt.
8. The enhanced paving shown at the project entries needs to be provided all the way to the curb line rather than squared off.
9. Is the area immediately north of Building 23 a detention basin? Is it usable or will it be fenced?
10. Sheets L-01 through L-04 of the Landscape Concept Plans show a lot of ground cover but very little (if any) shrubbery. This is likely just a plotting error.
11. For the landscape area enlargements (Sheets L-05 through L-08), provide a legend showing tree/shrub species types.
12. The open space area behind the commercial portion of the senior housing building won't get much light, especially if the adjacent property to the west is ever redeveloped. Make sure to select landscape species that can tolerate these conditions.
13. There is a large public water main running through the landscape plaza at the corner of Valley Boulevard and E. Valley Parkway (enlargement on Sheet L-06) which will impact the location of proposed trees and structures.

### Architecture

1. Provide updated color and material sample boards/sheets.
2. Provide color callouts on the elevation sheets.

3. Provide architectural plans for the recreation building, restroom building, and pool equipment room.
4. For the row homes and villas, identify the color scheme for each building on the architectural site plan.
5. *Varied roof pitches shall be provided and shown on roof plans. Roof plans have not been provided.*
6. *Buildings need more articulation. The plans show significant improvement in some locations, however additional modifications are needed in other locations, as described below:*
  - a. *Along the sides of the apartment buildings facing the main project driveway (specifically the west side of Building 1 and the north side of Building 23).*
  - b. *On the east side of Building 18.*
  - c. *On the east side of Buildings 8 through 11 and south side of Buildings 12 through 17.*
  - d. *On the garage side of the row homes.*
7. Identify the height of the buildings on the elevation sheets for all buildings. (They are not shown for the apartments and senior apartments.)
8. There are inconsistencies between unit plans, composite floor plan and elevations, with regard to locations of windows/doors on various buildings.
9. *Identify the location of A/C units.*
10. The following items need to be addressed on the plans for the apartments:
  - a. Identify the plan sheets consistently as either Buildings 1, 18, and 23, or A, B, and C.
  - b. *In apartment Building A composite (Sheet A-2), the stairs from the garage to the unit at the bottom right corner of the building do not align with those on the second floor.*
  - c. Provide inset/pop-out measurements for all elevations.
  - d. Sheets A-3.1 and A-3.2 both show the south and west elevations.
  - e. *Provide separate mezzanine plans for Building A and B composites. This was provided for Building A but not for Building B.*
  - f. *Identify how access is provided to the common areas in Building C, where doors/windows are, access to decks, etc. Also, provide more detail on how the ground floor interacts with the open space in front of it, and how it relates to the street edge to provide an area that is welcoming to pedestrians. Please clarify your response to this comment.*
  - g. *Describe how elevator access and connectivity is provided to each area in Building C. It is unclear if access can be provided to the third and fourth floors from more than one elevator, and if access to the common areas is available from the residential hallways without having to go to the ground floor and back up another elevator. Please clarify your response to this comment.*
  - h. The buildings include a lot of dark colors/materials. Lighter colors/materials shall be used in various areas.
  - i. Provide access between the upper floor of middle leg of the building and the remainder of the building.
  - j. Consider enlarging the Plan 1 units in areas where there is no need for access to the middle leg of the building.
  - k. *Identify the use of rooms and other areas which are not part of any units (examples: room next to elevator on first floor of Building A, and rooms at the end of the buildings on the first floor).*
11. The following items need to be addressed on the plans for the senior apartments:
  - a. The entrance to the building lobby needs to be more defined.
  - b. Provide inset/pop-out measurements on the building elevations, similar to what was provided for the apartment buildings.

- c. Verify that enough ventilation is provided from the garage, and identify the location of additional ventilation, if necessary. This is necessary at this time because it will significantly affect the exterior appearance of the building, and possibly the floor plan as well.
- d. Interior doorways are not shown in various areas. For instance, access to the common lounges and common deck, access between the lobby and garage, stairways at various locations, trash rooms, etc.
- e. The stairways on the first floor plan for units having direct access from Valley Boulevard do not appear to be match their locations on the second floor.
- f. Some areas of the building would benefit from additional windows or balconies, particularly on the north and west elevations, and the first floor along Valley Boulevard.
- g. Include a floor plan showing the stairway access for Plan 2.
- h. Is the parking area under the deck gated?

#### DESIGN REVIEW – PLANNING COMMISSION

The items below were discussed at the Design Review session at the October 8, 2019, Planning Commission meeting, and should be taken into consideration as necessary when making revisions to the project design. Comments below were identified by individual Planning Commissioners and are not collective recommendations by the Commission. Staff is providing this information as a summary of issues raised, and does not necessarily support all of them. That said, staff does believe that the session was helpful in identifying potential design changes that may benefit the project.

- Pedestrian access from the interior of the project site to the plaza on the corner of Valley Boulevard and E. Grand Avenue.
- Round-about/traffic circle at the Valley Boulevard/E. Grand Avenue intersection.
- Tower element:
  - a. Not accessible
  - b. Eliminate it
  - c. Too boxy
- Architecture should promote the project site's legacy as the home to a former fire station, City Hall (round building), and adobe
- Lack of density
- More commercial space needed; consider Milpitas example (from slides)
- Energy efficiency
- Nice design. Mixed use is good.
- Good mix of for-sale and for-rent units
- Rental units should meet condo specs
- Outdoor dining area on west parcel is good
- No live music should be permitted on tower deck
- More parks needed
- The dog park is a nice amenity
- Add bike parking and space for scooters/motorcycles

A letter from Ken Erickson, Architect, was provided to the Planning Commission at the Design Review session and has been enclosed with this letter. Other public comments provided to the Planning Commission were attached to the staff report prepared for that meeting, which can be found at the following link:

<https://www.escondido.org/Data/Sites/1/media/agendas/PC/2019/100819PCAagendaPacketA.pdf>.

#### TECHNICAL REPORTS

1. On the Fig Street Reclassification memorandum, identify the LOS D capacity for a Local Collector street.
2. On the Sewer Study, Page 1-1 (Conclusions and Recommendations), please include the square footage of commercial space that will be provided with sewer service.
3. On the Geotechnical Report, Page 2, Section 2 (Site and Project Description), the last sentence appears to include information about a different project (existing parking structure and hotel, and surrounding improvements). Remove this information.

#### **ENGINEERING COMMENTS**

##### REPEAT COMMENTS FROM PREVIOUS REVIEW

1. The widening of Grand Avenue along the project's frontage to Collector Street standard needs to be *clearly shown and labeled in all plan view sheets* of the Tentative Map. This street widening will require the removal of multiple large eucalyptus trees along E. Grand Avenue near N. Fig Street and these removals should be identified.  
Note: A 2-foot wide Public Utilities Easement can be granted along the project's E. Grand Avenue frontage instead of dedicating 2 additional feet of right-of-way to make up the standard 42-foot half-street right-of-way required for a Collector Street.
2. Although staff feels the segment of N. Fig Street from E. Valley Parkway to E. Grand Avenue has the potential to be classified lower to a Local Collector or even Residential classification, this has not been done yet and so a Specific Alignment Plan and/or Reclassification Study will need to be done and the results approved by City Council with the project and environmental.
3. A 20-foot radius right-of-way corner rounding at the northwest corner of E. Grand Avenue and N. Fig Street needs to be clearly shown and labeled in plan view on the Tentative Map.
4. The traffic signal modification required at the intersection of E. Grand Avenue and N. Fig Street to accommodate the required street widening and curb return reconstruction needs to be clearly indicated on the Tentative Map.
5. The direct pedestrian connectivity to N. Fig Street still needs to be clearly labeled and dimensioned for width on the Tentative Map.  
Note: Stairs should also be added in this location for a more direct pedestrian connection to N. Fig Street.
6. Need to provide on the Tentative Map a detail of the main signalized entrance/intersection at N. Hickory Street/E. Valley Parkway/Valley Boulevard showing proposed lane designations, widths, and alignments. This detail needs to include N. Hickory Street to beyond where E. Pennsylvania Avenue tees in.  
Note: No in-bound lane at this main signalized entrance is indicated in the few places on the Tentative Map and Conceptual Grading plans where striping and turn arrows are vaguely delineated; this will need to be changed and clearly shown on this requested intersection detail.
7. Need to provide sight distance lines at all project entrances.  
Note: Although site distance lines were added to Conceptual Grading Plans, their impact does not seem to be incorporated into the project design. Site distance must be achieved at all un-signalized intersections/entrances. If its determined that the main signalized entrance cannot be reasonably made to meet site distance requirements, then than "No Right Turn on Red" will need to signed and noted on the intersection detail described above. If no right turn is permitted on red, make sure this is analyzed when making revisions to the Traffic Impact Analysis.
8. The alley to the west of Building #24 (Lot 1) needs to be widened to 24 feet as part of this project and its right-of-way shown and called out to be dedicated accordingly.
9. All proposed retaining walls that remain an approved part of the project need to be constructed completely within the project property and nor shall their footings encroach into the public right-of-way.

10. Need to show complete dimensioning on the Tentative Map of all the private street/drive aisles and label the internal corner/curb return radii.
11. The details shown on the Conceptual/Preliminary Grading Plan and the Conceptual Site Plan need to be combined into the Tentative Map.  
Note: The new 30 scale is helpful but all the design components need to be consolidated and combined both into the Tentative Map in general but also together on sheets where possible to verify their compatible overlap and interactions.
12. Need to clearly show and label the extent and configuration of the raised medians in E. Grand Avenue required to control left turn movements in and around the project entrances.
13. The required traffic signal modification required at the intersection of E. Grand Avenue and Valley Boulevard needed to accommodate the realignment and reconfiguring of Valley Boulevard needs to be clearly shown and labeled on the Tentative Map and will be a project condition of approval.

#### NEW COMMENTS

1. Need to verify the condition of the existing 49"x33" CMPA storm drain pipe in E. Valley Parkway through which most of the project's storm water is conveyed offsite and if it is found to be in poor condition it will need to be lined or replaced. A note to this effect on the Tentative Map will suffice for entitlement.
2. The portion of the project draining to Fig St. (23.4 cfs) is proposed to do so via a connection to the existing 24" RCP storm drain in N. Fig Street while currently the drainage from the project site enters this system further downstream via inlets off of a 33" RCP storm drain. The existing 24" should be analyzed with these added flows, and if its capacity is exceeded then this pipe will need to be upsized and so noted on the Tentative Map.
3. The Modular Wetland Units proposed to meet the project's storm water treatment and retention requirements are shown on the Conceptual Grading Plans in an almost schematic fashion (MW and size (8'x16') and there are a few locations where this schematic plotting appears to overlap with an incompatible structure or improvement, and in the report the sizes vary significantly by basin/location. How treatment flows enter these units needs to be shown a little clearer on the Tentative Map both in plan view and in a detail for a typical curbside install.
4. Need to clearly show on the Tentative Map where the Modular Wetland designated in SWQMP to treat the Lot 1 (Building 24, Senior Housing) is located and how drainage is conveyed to it.
5. Need to show on the Tentative Map and in the SWQMP how and where the storm water in the Valley Boulevard and E. Grand Avenue road widening areas will be treated. (Green Streets or Modular Wetlands).
6. The multiple driveway entrances off the public alley to the Lot 1 Senior Housing Building need to be designed and shown as 24' wide modified (10' radius) alley type aprons instead of the standard dust pan aprons currently shown.
7. The entrances to the alley west of Lot 1 (Building 24, Senior Housing) need to be reconstructed as 24' wide alley type driveway aprons. The alley entrance onto E. Valley Parkway needs to be realigned perpendicular to E. Valley Parkway.
8. It is our understanding that portions/lots of the project will be mapped as condominiums while others will not be. The individual lots being created need to be clearly labeled, dimensioned, and areas provided and those Lots to be mapped to contain condominium ownership need to clearly note this and the number of units listed for each. All existing and proposed easements need to be clearly labeled with record info. or proposed type/grantee together with width and dimensions provided as needed.

VALLEY BOULEVARD REALIGNMENT AND LANE CONFIGURATION

Comments previously provided via email on September 6th:

1. Our latest review of the Traffic Impact Analysis still has a few substantial comments, corrections, and additions which might impact the Valley Boulevard design. TIA comments have been included with this letter.
2. The proposed 53' curb-to-curb distance, as depicted on Section G (facing northeast), is acceptable, however the lane widths should be adjusted as follows:

8.5-foot parking  
3-foot buffer  
12-foot eastbound drive lane  
12-foot eastbound drive lane  
2-foot buffer  
5- eastbound bike lane  
2-foot buffer  
8.5-foot parking  
Remove westbound bike lane.

3. The centerline of Valley Boulevard, as shown on the Conceptual Grading Plan, seems to shift as it crosses the E. Grand Avenue intersection, is then independent of the two eastbound thru lanes, and then further skews to join the true centerline of E. Valley Parkway approaching N. Hickory Street. A new centerline design controlling the new Valley Boulevard improvements and lane lines from south of E. Grand Avenue to east of the N. Hickory Street intersection and meeting the City's Local Collector (35 mph) Design Standards needs to be developed and clearly laid out on the Tentative Map together with and tied to the existing/historic centerline of Valley Boulevard so that the requirements to dedicate and vacate right-of-way along Valley Boulevard are clearly defined. It appears the realigned segment of Valley Boulevard improvements and lanes together with its new centerline need to shift about 4 feet to the east.
4. The existing E. Valley Parkway improvements and lane lines east of N. Hickory Street need to be clearly shown with enough offset dimensioning to verify how the new Valley Boulevard alignment and lanes carry on through here and join. As previously requested, the N. Hickory Street/Project Entrance/E. Valley Parkway intersection modification needs to be detailed showing all proposed and required lane alignments, widths, turn pocket needs, etc.
5. Adjust the project corners and curb return design at E. Grand Avenue and Valley Boulevard as needed to join the new Valley Boulevard alignment and improvements.
6. The Local Collector geometric and design speed criteria are acceptable for the Valley Boulevard alignment only; the roadway/lane widths shall be per comment no. 2 above.
7. The Valley Boulevard and E. Grand Avenue corner pop-outs and the bus/multi-mode curb line pop-out should extend into the buffers to near the thru lane (west side) and bike lane (east side) to add pedestrian area and shorten the pedestrian crossing at the Valley Boulevard/E. Grand Avenue intersection.
8. The public right-of-way width from curb face along/behind the parking areas on both sides of Valley Blvd. shall not be less than 10 feet and the public sidewalk shall be shown to utilize all of this right-of-way. The right-of-way width near the E. Grand Avenue/Valley Boulevard intersection and around the bus stop/multi-modal areas should be wider to help create the multi-modal areas and pedestrian plazas that the City has requested previously.
9. The information on the Tentative Map showing right-of-way dedications and street vacations needs to be checked as we believe much of what is depicted as dedication near the project entrance at E. Valley Parkway has already been dedicated. Existing street vacations shall be labeled with resolution and recording information.



10. The bus stop area needs to be extended to accommodate a loading zone for at least two ride share vehicles and start a min. of 50 feet from the E. Grand Avenue curb return. The first parking space on the west side of Valley Blvd. should also be shifted to a minimum of 50 feet from the E. Grand Avenue curb return. It appears additional parallel parking spaces could be created on both sides at the north end of Valley Boulevard, as it approaches E. Valley Parkway.

#### DRAINAGE STUDY

Incorporate revisions identified above.

#### SWQMP

Comments in other sections of this letter above must be incorporated into the revisions to the SWQMP, as well as addressing the following issue:

1. DMA 12 (Lot1): Please clarify use of dispersion areas and how the building runoff is proposed to be routed to the dispersion areas/tree wells.
  - a. Provide a preliminary detail of tree well area(s).
  - b. Discuss how the landscaping and other proposed areas meet the qualifications of impervious area dispersion per Fact Sheet SD-5.
  - c. Is the landscape plan coordinated with the proposed storm water management?

The following corrections can be made when the PDP SWQMP is submitted at final engineering:

1. Step 6.2: Structural BMP Checklist
  - a. All Modular Wetlands BMPs should be correctly identified as Proprietary Biofiltration (BF-3) on each Structural BMP Checklist page (BMP ID Nos MWS-1 to MWS-11).
  - b. If the tree wells are being utilized as site design to effectively eliminate the DCV for DMA 12, they do not require a Structural BMP Checklist page. However, it is critical that these site design BMPs are maintained along with the other project Site Design and Structural BMPs.
2. Attachment 1e: Clearly identify the locations of the BMPs on each page in the DMA Mapbook.

#### TRAFFIC

Comments from the Traffic Engineering Division have been included with this letter as a separate document.

#### **UTILITIES COMMENTS**

##### GENERAL

1. Show all existing water/sewer/storm utilities and reference record drawing numbers. (This is a repeat comment). Existing water and sewer mains on-site should be shown on plans and not referenced only in a note. Existing water to be abandoned shall be abandoned at the main per approval by Utilities Engineer. Existing sewer to be abandoned at the property line and to be plugged at the main per approval by Utilities Engineer. Clarify on plans.
2. Sheets 2 & 3: Show all proposed water/sewer utilities in street sections. Label ROW/PUE. (This is a repeat comment).
3. Show all ROW/PUE limits on plan view (Sheet 6).
4. General and Sheet 9: Distinguish between public and private storm drain. Where possible, private storm drains should either be outside of the ROW/PUE or closest to the ROW/PUE edge. Private storm drain shall not split public water and/or sewer utilities.
5. Street widths do not match from sheet to sheet. Please clarify.
6. There shall be no permanent structures, including retaining wall footings, within the ROW/public utility easements.

## SEWER

1. Submit revised sewer study reflecting correct flows used per City Sewer Standards.
2. Per City Sewer Standards, sewer main shall be designed and constructed on centerline.
3. Show how each building will sewer to the public main. Show all proposed 6" private sewer. Private 6" sewer laterals serving more than 4 legal lots shall connect to the public main at a manhole.
4. A minimum 6" sewer lateral is required for commercial use.
5. No trees or deep rooted bushes within 15' of sewer mains.
6. Sheet L-01: No trees or deep rooted bushes within 10' of sewer laterals.
7. Sheet 9: Show existing sewer in the alley and clarify how Lot 1 will sewer. A minimum 6" sewer lateral is required for senior housing. 6" sewer lateral must connect to a minimum 8" sewer main.
8. Sheet 9: Add private sewer to legend.

## WATER

1. Existing 6" water main in Grand Avenue shall be upsized to a 12" water main from the intersection of Grand Avenue & Hickory Street to across the property frontage to the intersection of Grand Avenue & Fig Street. (Sheet 1: Edit legend to indicate 12" main unless otherwise indicated.)
2. Show all existing and proposed fire hydrants approved by the Fire Department.
3. All proposed hydrants shall be served by a minimum 8" water main.
4. Additional water main looping may be required depending on fire hydrant location.
5. Show DCA, FDC, PIV locations.
6. All existing fire hydrants and DCAs must meet current COE Water Standards (Add Note to Plans.)
7. All existing water services to be re-connected to new water mains shall be new services to the meter.
8. All existing fire hydrants to be re-connected to new water mains shall be new hydrant laterals to the hydrant.
9. Sheet 9: Water service to Lot 1 may not connect to the transmission main.
10. Sheet 9: Add DCA, FDC, PIV to legend.

## **FIRE DEPARTMENT**

1. Show all slopes, grades, cross slopes on access roadways.
2. Clarify construction type and square footage of each building in order to verify / provide minimum fire flow and hydrant spacing requirements.
3. Show fire hydrants every 350 feet. This distance may need to be reduced based on fire flow requirements. Provide additional hydrants.
4. Show all FDC/PIV locations. Each building will require its own FDC/PIV, and large buildings may require multiple.
5. FDCs shall be within 40 feet of a hydrant.
6. Clarify building heights for each buildings. Note that all mid-rise buildings shall comply with Section 320, mid-rise ordinance, and NFPA 13 & 72 shall be followed. Conditions will require that the buildings provide emergency voice alarm signaling, fire command center, annunciation ID, etc.
7. Show all turning radii on every corner (minimum inside radii) and provide fire turnarounds on access roadways over 150 feet in length.
8. Provide fire access roadway within 150 feet of the farthest wall and farthest projection. Some buildings do not meet this requirement.
9. Conditions of approval will require all buildings to comply with Section 510, emergency responder radio coverage.
10. Provide an adequate water supply and an approved all-weather access roadway prior to any combustible being brought to the site. Note this on the plans.

October 21, 2019

11. Minimum roadway width is 24 feet wide and shall paved in an approved all-weather surface able to hold the weight of the fire apparatus, 75,000 lbs. Clarify surface materials. Some access roadways (building on Valley Boulevard) may need to widened.
12. Provide vertical clearance on all roadways. Minimum clearance is 13, feet 6 inches.

**CONCLUSION**

Please revised the project plans and technical documents in order to address the items and information identified above so that staff can complete its review of your application. Note additional comments may be identified based on review of more-detailed and complete project plans and other submittal materials.

Please provide twelve full-size copies (folded) and one reduced copy (8 ½" x 11") of the revised plans, along with an electronic copy (preferably on a flash drive) incorporating responses to the items described above. *A copy of this letter with notes identifying where the requested information can be found shall accompany your resubmittal. Please also provide a response to the comments provided by the Traffic Engineering Division.*

If you have any questions regarding this information contained in this letter, please feel free to contact me at (760) 839-6203, or via email at [afinestone@escondido.org](mailto:afinestone@escondido.org).

Sincerely,



Adam Finestone, AICP  
Principal Planner

Enclosures

cc: Bill Martin, Director of Community Development  
Mike Strong, Assistant Planning Director  
Owen Tunnell, Assistant City Engineer

10-15-19

TO: Adam Finestone  
 CC: Owen Tunnell  
 FROM: Miriam Jim  
 SUBJECT: Palomar Heights; PHG 18-0049; ENV 18-0009

Here are the TIA (dated Sept. 13, 2019) review comments from Traffic Engineering. (2nd Review)

Reference	Review Comments
Land Use	TIA assumed commercial space of up to 7,000 SF. However, the latest Tentative Map shows 12,000 SF of commercial space. The uses would include, café, a gym, office space, and a sit down restaurant. Please update the TIA accordingly.
Figure 2-3	The site plan shows the main driveway from the project is a one-way exit only at Hickory St/Valley Pkwy intersection. Is that correct? The TIA assumed two-way access at this driveway. Please confirm and revise TIA if needed.
Table 3-1	Please use the count provided for N. Hickory Street segment.
Figure 7-1	Need to discuss the trip distribution. The revised figure still is not consistent with the regional trip assignment provided to the City or as described in the report. Trip heading east on Valley Pkwy should be higher, 32% trip going on Hickory is high. As mentioned in previous comment, traffic from Grand Ave would likely use Ivy St to get to Valley Pkwy to head west but no trip was assigned on Ivy St. Also it was requested to have some trips distributed to Pennsylvania Ave from Hickory St. based on existing traffic pattern but it was not addressed. Prior to completing the next TIA report, please provide the revised trip distribution for review and approval first.
Section 12.3.3	Bus service is provided by NCTD not MTS.
Table 12-2 VMT	How was the VMT calculated for the base year? Text refers to City's calculations, however, COE has not established a VMT methodology yet. It would be helpful to provide some detail on how the VMT was

Reference	Review Comments
	estimated. Why the number of residents and total trips in the project site are so low on Table 12-2?
Driveway Analysis	From previous comment: Please provide lane configuration of the driveway from project site at Hickory and Valley. The analysis should help identify the lane configuration and the left/right turn pockets and their required storage length.

TUESDAY - SEPTEMBER 25, 2018

CITY: ESCONDIDO

PROJECT: PTD18-0928-01

HICKORY ST - VALLEY PKWY TO WASHINGTON AVE

AM Period	NB	SB	EB	WB	PM Period	NB	SB	EB	WB		
00:00	2	3			12:00	31	34				
00:15	1	0			12:15	37	33				
00:30	4	5			12:30	26	32				
00:45	3	10	4	12	22	12:45	26	120	40	139	259
01:00	4	2			13:00	21	23				
01:15	2	2			13:15	25	22				
01:30	2	1			13:30	32	24				
01:45	1	9	0	5	14	13:45	27	105	35	104	209
02:00	3	0			14:00	46	38				
02:15	3	4			14:15	48	40				
02:30	0	0			14:30	38	47				
02:45	2	8	3	7	15	14:45	43	175	38	163	338
03:00	1	0			15:00	64	37				
03:15	0	0			15:15	42	32				
03:30	0	0			15:30	53	27				
03:45	2	3	2	2	5	15:45	57	216	41	137	353
04:00	3	1			16:00	62	47				
04:15	1	4			16:15	68	55				
04:30	1	2			16:30	57	49				
04:45	3	8	9	16	24	16:45	62	249	47	198	447
05:00	7	8			17:00	72	43				
05:15	6	14			17:15	60	33				
05:30	8	16			17:30	68	43				
05:45	18	39	25	63	102	17:45	51	251	41	160	411
06:00	13	39			18:00	40	39				
06:15	18	41			18:15	45	50				
06:30	23	42			18:30	45	36				
06:45	26	80	46	168	248	18:45	47	177	35	160	337
07:00	26	51			19:00	35	38				
07:15	19	51			19:15	40	31				
07:30	32	52			19:30	34	43				
07:45	22	99	72	226	325	19:45	23	132	28	140	272
08:00	19	57			20:00	24	21				
08:15	22	49			20:15	30	24				
08:30	19	26			20:30	21	14				
08:45	16	76	44	176	252	20:45	19	94	26	85	179
09:00	25	44			21:00	24	19				
09:15	27	33			21:15	23	22				
09:30	19	34			21:30	17	9				
09:45	23	94	30	141	235	21:45	12	76	15	65	141
10:00	21	32			22:00	20	9				
10:15	24	21			22:15	8	15				
10:30	33	37			22:30	13	11				
10:45	27	105	29	119	224	22:45	9	50	8	43	93
11:00	29	32			23:00	15	9				
11:15	29	24			23:15	7	5				
11:30	34	29			23:30	6	8				
11:45	40	132	25	110	242	23:45	8	36	3	25	61
<b>Total Vol.</b>	<b>663</b>	<b>1045</b>		<b>1708</b>		<b>1681</b>	<b>1419</b>				<b>3100</b>
						<b>NB</b>	<b>SB</b>	<b>Daily Totals</b>			
						2344	2464	<b>EB</b>	<b>WB</b>	<b>Combined</b>	
										<b>4808</b>	
<b>Split %</b>	<b>AM</b>				<b>PM</b>						
	38.8%	61.2%		<b>35.5%</b>	54.2%	45.8%					<b>64.5%</b>
<b>Peak Hour</b>	11:30	07:15		<b>07:00</b>	16:45	16:00					<b>16:15</b>
<b>Volume</b>	142	232		<b>325</b>	262	198					<b>453</b>
<b>P.H.F.</b>	0.89	0.81		<b>0.86</b>	0.88	0.90					<b>0.92</b>

**Palomar Heights**  
Palomar Health Downtown Campus

October 2, 2019

Mr. Adam Finestone  
Principal Planner  
City of Escondido

RECEIVED  
OCT 15 2019  
PLANNING DIVISION

Dear Adam,  
Please forward these observations and comments to the Planning Commissioners.

The approved Downtown Specific Plan (DTSP) was developed over an eight-year period, adopted on August 7, 2013. During this time, residents, downtown business owners, Downtown Business Association, Chamber of Commerce, City staff, Planning Commission and City Council members gave input, discussed and debated with the goal of updating the vision for Downtown.

This effort recognized and respected the historic character of downtown but also considered the future, envisioning an attractive, pedestrian friendly, economically vital city center providing social, cultural and residential focus.

The purpose of DTSP is to direct Downtown development, based on its principles and guidelines to the desired outcome. All proposed projects in the Downtown are reviewed and assessed based on this thoughtful document to ensure that in the future the Downtown will have fulfilled the vision of those that contributed.

Palomar Heights exhibits reviewed are dated September 10 and 11, 2019 and the following comments and observations are based on the approved DTSP.

**General Observations:**

The proposed suburban project, necessarily employs site and grading designs that ignore existing site topography and the surrounding context resulting in significant grade change along street edges and public sidewalks. This approach, along with the fact that Buildings 1, 18, 23 & 24 propose parking garages on the ground floor level, isolates the project both physically and visually from the surrounding neighborhoods and does not provide the pedestrian environment which is a central goal of the DTSP.

See DTSP: Page II-12, III-1 (2<sup>nd</sup> paragraph), Page III-6 2 a)

**Building 1 Apartments:**

- Building One is 325' +/- in length and five stories in height. Along East Valley Parkway, there are retaining walls (2'-14' in height) and slope banks that result in the ground-floor being an average of 11' above the sidewalk
- Distances from building to street and sidewalk range between 20' and 35' +/- and at one point 10'
- Apartment units are not located on the street level or ground floor, in fact, with the exception of stairs to some second-floor units, the ground floor is a parking garage. Some second-floor units facing East Valley Parkway do have doors to their stairs but will appear secondary rather than the unit's entry door.
- Considering the length, elevation above street level and distances from building to the sidewalk and units located above the ground level, the proposed design does not provide the pedestrian environment which is a central goal of the Downtown Specific Plan.

See DTSP: Pages I-2 9., III-1 (2<sup>nd</sup> paragraph), III-7 b

**The "Villas" and "Rowhomes"**

- The Villas and Rowhomes are automobile-orientated suburban solutions. With surface parking, drive aisles and driveways, these buildings (the "Villas" in particular) will be surrounded by large areas of asphalt.
- The majority of Villas have unit entries located on drive aisles where cars access garages. The landscaping in this area amounts to small pockets every 20'. This space, with 3-story buildings on either side, is essentially an alley, which does not provide pedestrian oriented entries.
- Along Fig Street, per site section C-C, with terraced retaining walls of 15' in height and slope banks, the building ground-floor is approx. 20' above the adjacent sidewalk. Adjacent to Grand Avenue, some buildings are approx. 7' away from retaining walls and as much as 8' below street level.
- Based on these observations the proposed design does not provide the physical connection to the surrounding neighborhood nor create the pedestrian environment which is a central of the Downtown Specific Plan

See DTSP: Pages I-2 9., III-1 (2<sup>nd</sup> paragraph), III-6 2) Guidelines a) & III-7 b

**Building 18 Apartments**

- Similar to Building 1, apartment units are not located on the ground floor, in fact, with the exception of stairs to some second-floor units, the ground floor is a parking garage. Some second-floor units facing Grand Ave. do have doors to stairs but will appear secondary rather than the unit's primary entrance, doing little to activate the street edge and public realm.
- Based on these observations the proposed design does not provide the physical connection to the surrounding neighborhood nor create the pedestrian environment which is a central of the Downtown Specific Plan

*DTSP Page III -7 2) Guidelines: d*



**Building 23 Apartments**

- Building 23 is 430' +/- in length, five stories in height and elevated between 5' -10' above Valley Boulevard.
  - Similar to Buildings 1 and 18, apartment units are not located on the street level or ground floor and, with the exception of stairs to some second-floor units, the ground floor is the parking garage. Some second-floor units that face Valley Boulevard have doors to their stairs which will have the appearance as a secondary rather than the primary entry point.
- Based on these observations the proposed design does not provide the physical connection to the surrounding neighborhood nor create the pedestrian environment which is a central of the Downtown Specific Plan  
*DTSP Page III -7 2) Guidelines: d*
- The intersection of Grand Ave. & Valley Blvd. is one of the most important in Downtown. What is built here will be seen on Grand Avenue from blocks away and will contribute to the larger visual experience.
- The proposed tower would serve the project and Downtown better if located so as to have a strong connection to the ground plane rather than a roof top. And a more solid, slender design, would read as timeless, have a strong sense of permanence and better seen from further away.

**Building 24: Senior Apartments**

- Similar to Buildings 1, 18 and 23, apartment units are not located on the street level or ground floor. With the exception of stairs to some second-floor units and the small commercial space, once again, the ground floor is a parking garage. Some second-floor units facing Valley Boulevard have doors to their stairs, but with the proximity of the elevator, it seems likely that the doors and stairs will be little used. The small lobby appears to have solid walls with only a door on street side.
  - On the front elevation, several openings for garage ventilation are shown. With the garage floor below the sidewalk level, there will be views into the parking area, which is strongly discouraged in the DTSP
- Based on these observations the proposed design does not provide the physical connection to the surrounding neighborhood nor create the pedestrian environment which is a central of the Downtown Specific Plan  
*DTSP Page III -7 2) Guidelines: d*

Conclusions:

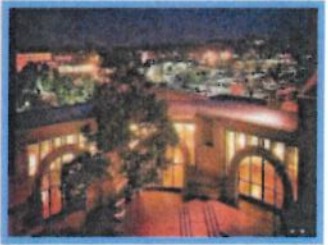
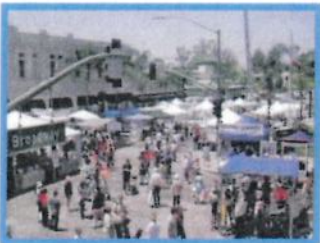
- This site is a once - a- generation opportunity, any project built here, will be there for a generation.
- The project as proposed, is a forced fit, it's a suburban solution that can be found on any flat site, anywhere. It does not add to the character, scale and established walkable rhythm of downtown, in fact, it's the antithesis of what was and envisioned by the Downtown Specific Plan.
- Every building matters, each one (good or bad) is part of the visual fabric that expresses Escondido's character and values. We should not accept, just for the sake of adding more housing, compromised site planning, grading design and architecture.
- We live in an age of indistinguishable architecture that erodes the differences and distinctiveness of cities and neighborhoods. This site, our historic downtown and Escondido residents deserve a project designed specifically for this site, in a unique neighborhood and city.
- We have a thoughtful Downtown Specific Plan that, by employing time tested planning principles, honors the scale and rhythm of the historic character of downtown, yet embraces this current place in time and the future. Again, all proposed projects in the Downtown must be reviewed and assessed based on this document to ensure that in the future Downtown will have fulfilled the vision of the DTSP. If plan is not the measure of whether a project should be approved or denied, what is?

Respectfully,  
Ken Erickson, Architect





*ESCONDIDO*  
*Downtown Specific Plan*



*Adopted on August 7, 2013*  
*Resolution 2013-85*



*File No.: PHG 13-0018*

# Downtown Specific Plan

## VISION STATEMENT & GOALS

### B. THIS DOCUMENT'S PURPOSE

The Downtown Specific Plan document provides a comprehensive plan for land use, development regulations, development incentives, design guidelines, pedestrian and mobility improvements, and other related actions aimed at implementing the strategic goals for Downtown Escondido as set forth in the General Plan Goals and Policies. As downtown transitions to a more urban environment involving taller structures, and buildings constructed along the street edge, focused efforts are required to ensure that the unique character is maintained and strengthened. The Vision, Goals, Principles and Guidelines promote a balance of uses, sensitive design techniques, and enhanced pedestrian opportunities.

### C. DOWNTOWN SPECIFIC PLAN STRATEGIC GOALS

1. An economically viable Downtown with an appropriate mix of retail, office, residential, entertainment and cultural uses.
2. A local and regional destination for specialty shopping, dining, nightlife, employment, culture, and the arts.
3. A vibrant and exciting environment with land uses that foster an "18-hour" atmosphere, in addition to areas that provide mixed use, office employment and high-density residential opportunities.
4. Development and signage that strengthen the character of Downtown and are architecturally compatible with the existing urban fabric.
5. Street-level and human-scale design elements in new and remodeled developments that improve pedestrian orientation.
6. Preserved historically significant sites and structures that enhance the character of Downtown.
7. Pedestrian-oriented, ground-floor, specialty retail and restaurant uses on Grand Avenue that reinforce and expand its unique character.
8. Higher residential densities in key locations that support Downtown non-residential uses.
9. A pedestrian environment that provides connections, convenient access and opportunities for alternative modes of transportation.



# Downtown Specific Plan

## LAND USES

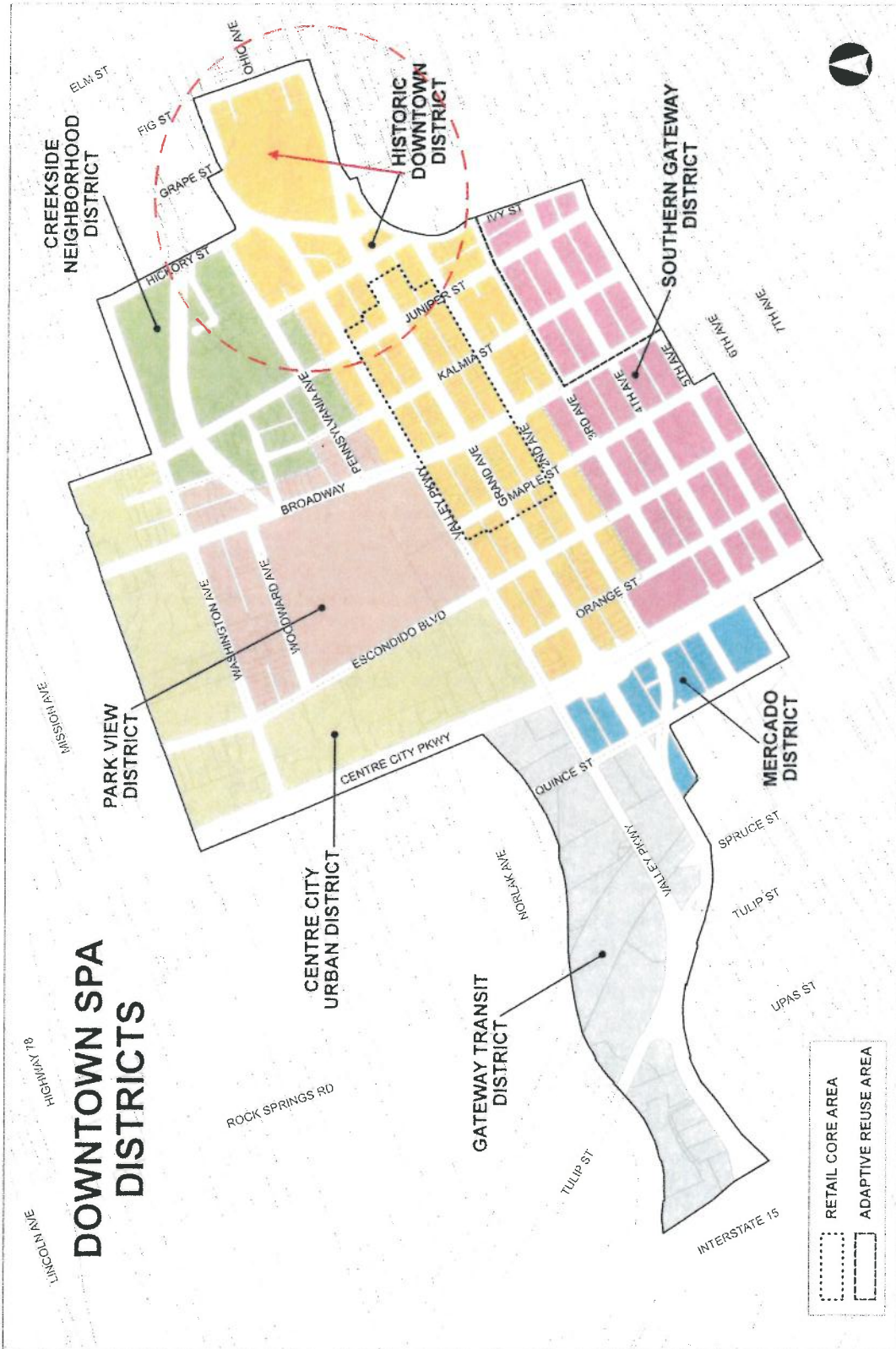


FIGURE II-1

# Downtown Specific Plan

## LAND USES

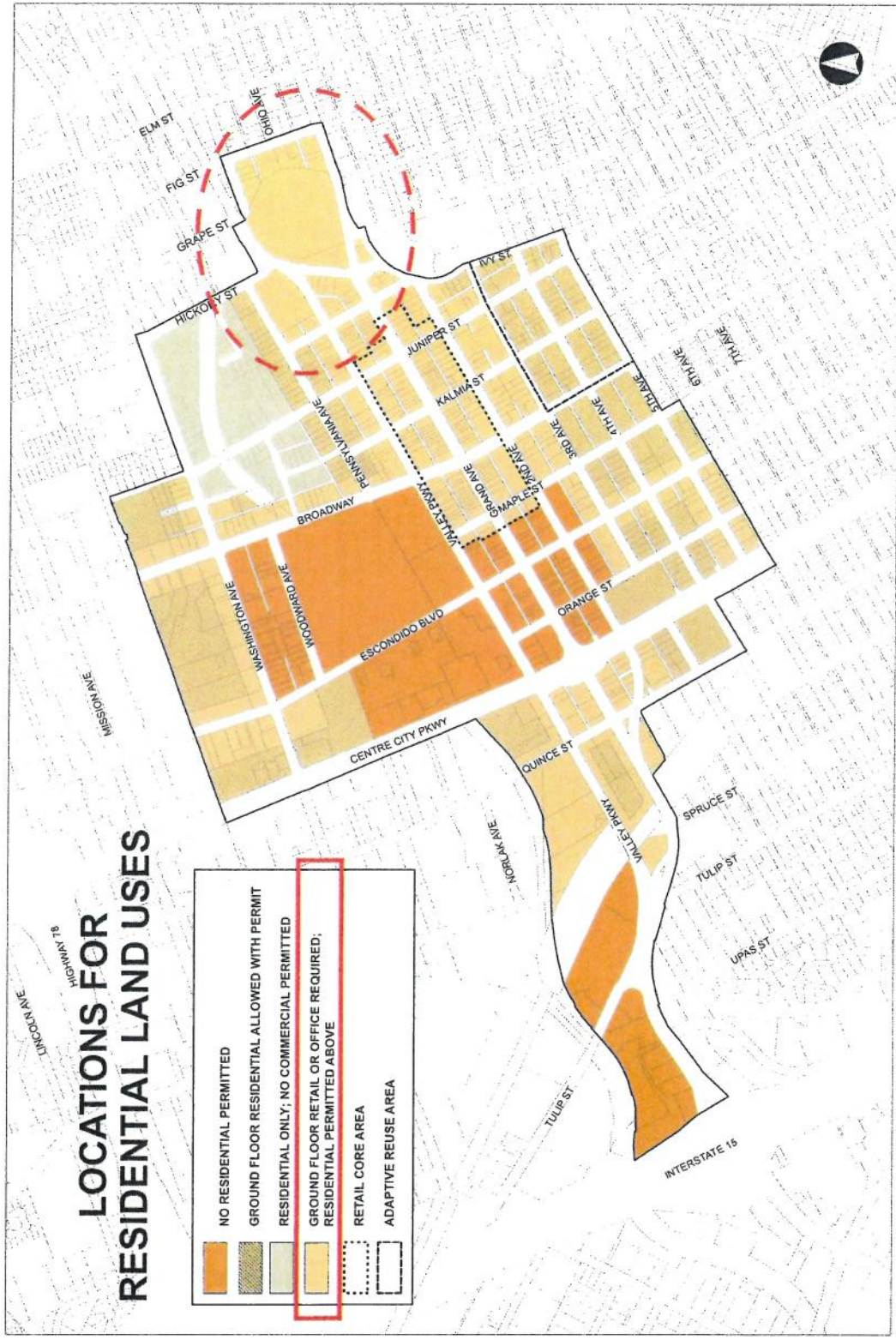


FIGURE II-4

# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

### III. DESIGN POLICIES, STANDARDS, AND GUIDELINES

#### A. PURPOSE

There exist certain elements of good urban design in the creation of public and private spaces that make up the fabric of a downtown. This document seeks to identify those policies, standards and supportive guidelines, to explain them in a way that is clear, and to give direction on their use. Proposed projects should preserve, respect and promote the existing character, scale, and the patterns of downtown Escondido. In its transition to a more urban environment, efforts should be made to capitalize on the City's unique history, geography and climate.



Downtown's urban atmosphere is envisioned to address the increased intensity of pedestrian and vehicular activity. Emphasis is focused on facilitating an enjoyable 'walkable' experience. Streets accommodate wider sidewalks, transit vehicles making multiple stops, pedestrian crossings, buildings closer to the right of way, and other features that affect vehicular and pedestrian traffic.

Downtown design policies, standards and guidelines embrace strategies representing a shift in focus from 'how to develop vacant land' to 'how to reinvest in existing neighborhoods.' Escondido's General Plan accommodates and guides urban residential growth downtown in order to preserve densities in established single family areas. This strategy helps ensure housing options for all residents and capitalizes on the city's infrastructure investments.

The Downtown SPA also incorporate policies ensuring that transportation planners and engineers consistently design and operate the entire roadway with safety and accessibility of all users – bicyclists, transit vehicles and riders, automobiles, and pedestrians of all ages and abilities, as well as goods and services. These policies shift Escondido's orientation away from building streets primarily for automobiles and facilitate the broader vision of directing Downtown's and the City's circulation system toward a truly 'multimodal' transportation network.

# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

The goal of this chapter is to encourage appropriate urban design solutions, increase the level of quality projects and to provide the community with a future environment that is as rich as its past. There might be points of discussion on any project for which there is no one clear solution. Preserving the character of Downtown Escondido, along with the area's overall vision, should be the fundamental goal of the design process.

### 1. SMART GROWTH POLICIES, STANDARDS, AND GUIDELINES

#### a. "Sense of Place" Policy:

**Promote a "sense of place" and enhance the community's historical context.**

Project architects and planners should strive to understand the history and character that make Downtown a unique place and how their project design for a specific site would contribute to Escondido being "Somewhere vs. Anyplace."

#### 1) Standards:

- a) The street level shall be enhanced as an inviting place for pedestrians by providing features that are visually interesting and human in scale including seating, public art, outdoor displays, historical plaques and landscaping.
- b) Historically significant facades and architectural features visible from the public right-of-way shall be preserved. Replacement elements on historic buildings shall replicate the original elements as closely as possible in terms of materials, profile, and detail which add to the building's character and the down-town overall. Replication should be based on historic evidence.
- c) Structures proposed for adaptive re-use shall retain the appearance of their original use, particularly if the structure is deemed historically significant. Landscaping, lighting, signage, parking, and other exterior structural additions/modifications, shall be designed to not impact surrounding residential uses.



#### 2) Guidelines:

- a) Proposed projects should preserve, respect and promote the existing character, scale, architecture and the patterns of the historically significant downtown.



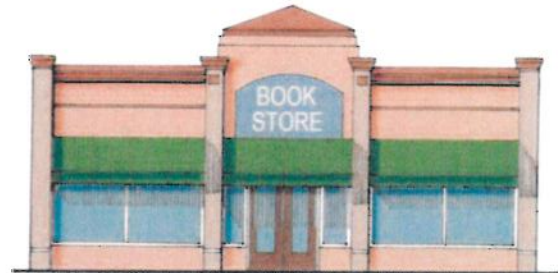
# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

- b) Larger projects should be designed to emulate the existing rhythm and scale of the downtown to minimize expansive inactive storefronts.

**b. Building Materials Policy:**  
**Utilize appropriate urban-style building materials, textures, colors, signage, lighting, massing and balance.**

Well-designed building architecture and signage shall respect and relate to its surroundings in scale, mass, color, use of texture, character, materials, and asymmetrical or symmetrical balance,



EMPHASIS THROUGH MASSING

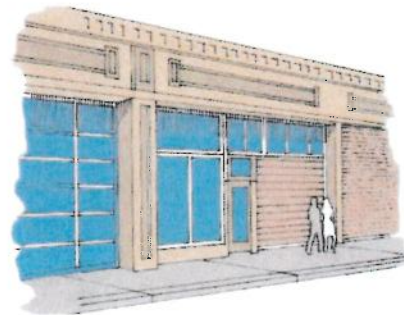
**1) Standards:**

- a) Exterior building materials typically associated with urban environments that are durable and timeless (concrete, brick, stone, metal, glass, etc) shall be primarily used.

THE DIFFERENT FINISHES OF A MATERIAL CREATE TEXTURE

- b) Colors for building wall surfaces, trim areas, and awnings shall be coordinated and non-garish. Consultation with a color specialist to ensure a harmonious palette is encouraged.

- c) The architectural design of detached buildings shall complement the main structure through the use of compatible details, materials and colors.



TEXTURES PROMOTE INTIMATE SCALE

- d) Well-designed and appropriate exterior lighting shall be incorporated to provide security, visual comfort for pedestrians, and enhancement of façade details.

- e) Indirect lighting shall be incorporated where appropriate to minimize glare into pedestrians' eyes. Building lighting that blinks, changes, or is otherwise distracting shall be avoided.

- f) Buildings shall have architectural features and patterns that reflect and reinforce the scale; mass, proportion, rhythm, and attention to detail established by existing well-designed structures, as well as provide visual pedestrian-scale interest, reduce massive visual effects (such as avoiding large blank walls, step-back building facades for upper floors, etc.).

# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

### **c. Building Orientation Policy:**

**Orient buildings toward the street to establish a strong connection with the ground plane and immediate surroundings, and to appropriately reflect the building's purpose and use.**

A continuous 'street edge' frames and defines the public space, transforming the street into an enjoyable outdoor public space. Buildings with transparent store fronts at the sidewalk engage the pedestrian's interest, attention and curiosity. Conversely, buildings set far back from the street erode the "street edge," disconnect the pedestrian from the building's use and activity, as well as prevent the pedestrian from enjoying attractive building details.



# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

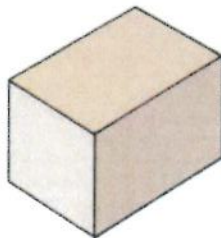
### 1) Standards:

- a) Building entry points shall be clearly visible. As required by the Specific Plan District, approximately 70% of the building wall along the street shall contact, or be in close proximity to, the street edge to provide an urban look and help activate the street by avoiding the use of columns with cantilevered over-hangs and/or porches along the street edge.

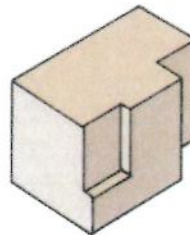


- b) Elements that vary façade planes and create a visual play of light and shadow shall be incorporated along the street edge. Long, uninterrupted, horizontal surfaces and “box-like” appearances shall be avoided. Display windows, store entrances, upper windows and other architectural features are encouraged in these locations.

(BOX-LIKE APPEARANCE)  
DISCOURAGED

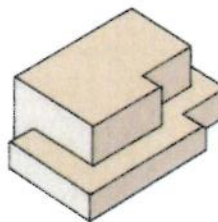


(VERTICAL ARTICULATION)  
ENCOURAGED



- c) Building setbacks that transition from the public right of way shall incorporate hardscape materials (pavers, colored / stamped concrete, raised planters, etc.) that are compatible with public right of way improvements to promote comfortable pedestrian activity and accommodate seating and shade as well as protection from the elements.

(VERTICAL ARTICULATION)  
ENCOURAGED



### 2) Guidelines:

- a) A building's main front façade should not be set back far from the street. The majority of a building footprint should, especially at the street edge, contact the ground plane with a design that reflects the retail, office, or residential use.

# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

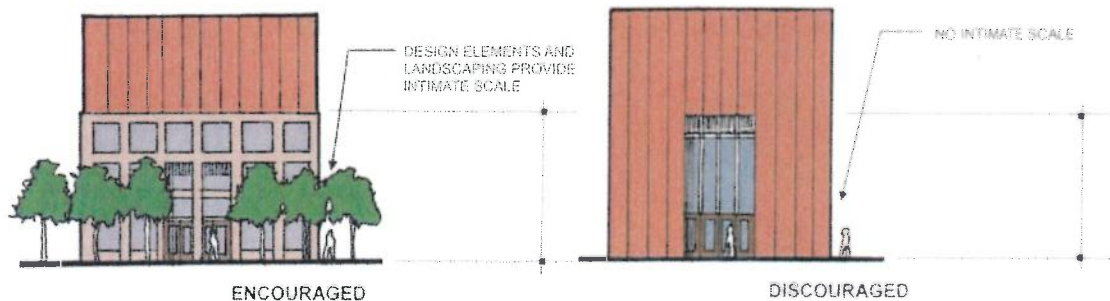
- b) Pedestrian-oriented storefront windows should be located on the street edge as well as pedestrian-scale signage, canopies, and awnings. Doors can be offset slightly away from the street to increase window area.
- c) Changes in paving, hedges and walls also should be used to define the street edge in addition to building façades.

d) Buildings that are elevated to accommodate 'tuck-under' parking are strongly discouraged because they 'disconnect' the building's users from the ground, street and neighborhood.

### **d. Pedestrian-Oriented Environment Smart Growth Policy:**

**Incorporate higher building profiles and pedestrian-scaled features that strengthen a compact, pedestrian-oriented environment.**

A compact, walkable community is established by incorporating narrow storefronts and lots with increased frequency of entry points in both commercial and residential buildings that limit long expanses of inactivity along the street.



### **1) Standards:**

- a) Projects shall incorporate high-profile and vertical design features sufficient to portray a desirable urban quality and comfortable public realm with adjacent buildings.
- b) The ground-floor portion of buildings shall be architecturally distinguished from the upper façade to form a visual base for the building and to create an intimate scale for the pedestrian environment.
- c) Side and rear facades shall be articulated in a comprehensive design that is compatible with the design of the front façade.
- d) Arcades built over the sidewalk or in front of store windows shall be high enough and open enough not to obscure display windows.

# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

- b) Landscaping shall not be placed so as to screen the doors and windows of units from the street or from walk-ways leading from the street to the dwelling-unit entries.
- c) On-site parking shall not diminish the defined street edge or detract from the pedestrian experience.
- d) Development shall address residential, guest, and commercial parking needs on-site as well as the appropriateness of gated and/or reserved parking.
- e) Exterior lighting fixtures in parking areas and driveways shall utilize cutoff shields, or other appropriate measures, to conceal the light source from adjacent uses and rights-of-way.
- f) Parking lots shall be landscaped to soften their area and provide a visual buffer. A combination of trees and shrubs in a landscaped strip or planter creates an effective separation where a parking lot abuts a public sidewalk.



- g) Structured parking shall create a visually attractive and active street edge to enhance the pedestrian experience in the following manner:
  - i. Parking structures shall generally be limited to extend no more than one-half story above grade adjacent to the sidewalk, or shall be developed below street level.
  - ii. Parking garage facades shall complement the area's existing architecture.
  - iii. A majority of the parking structure's street frontage shall include sufficient retail or commercial space in non-residential areas to accommodate a variety of pedestrian-oriented uses, or construct the parking structure at the rear of the building.
  - iv. Vehicular entry points into garages should be minimized and located to minimize pedestrian exposure.

### 2) Guidelines:

- a) Public, semi-private, and private ground-level spaces should be defined by utilizing low walls, landscaping, stoops, porches, and decorative paving.

# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

- b) Features that can protect pedestrians from inclement weather (umbrellas, awnings, canopies, recessed entries, etc.) are encouraged and should be designed as a building's integral features.
- c) On-site parking should be located at the rear of buildings and be oriented in a manner that facilitates surveillance.
- d) Tandem spaces may be appropriate in residential developments.

### **f. Housing Smart Growth Policy:**

**Include a variety of housing types and densities to strengthen residents' vested interest in the success of downtown.**

Mixed-use and residential development involving a variety of densities creates vibrancy, provides more housing choices, and reduces the need for automobile dependency. Such variety provides residents opportunities for transitioning to different housing types within the same community and balances economic health.

### **1) Standards:**

- a) The residential density for mixed-use developments shall consider the planned urban vision as well as the existing development pattern as a means to establish compatibility.
- b) Residential development shall be a minimum of two stories and shall maintain a strong edge similar to commercial buildings.
- c) Residential projects, with portions of its underground parking extending above the sidewalk line, shall be designed so that the street edge is still attractive and inviting to pedestrians.
- d) Mixed Use commercial uses wrapping around street corners and architectural details that are differentiated on upper elevations shall be incorporated to establish a strong retail edge.
- e) Shopkeeper development shall be designed for owners to operate their businesses while living in the attached unit, which also provides separate exterior access to the residential portion of the unit.



# Downtown Specific Plan

## DESIGN POLICIES, STANDARDS & GUIDELINES

- f) The non-residential space of shopkeeper units shall be attached and provide access to the main dwelling unit, which shall be located above and/or behind the non-residential space. Areas devoted for residential use shall not be used as space for conducting business with customers or clients.
- g) Artisan loft and/or shopkeeper units shall not be designed to accommodate leasing or purchasing either the residential or commercial space by separate tenants.
- h) Mixed use retail space shall be developed with sufficient interior areas and ceiling heights to accommodate a wide variety of uses. Storefront widths of 25-30 feet with 42-45+ foot depths (inclusive of handicap accessible restroom areas), and 12- to 13-foot-high ceilings are considered to be sufficient.
- i) Shopkeeper commercial spaces shall be developed with sufficient interior areas and ceiling heights to accommodate a variety of businesses. Shopkeeper widths of 20-25 feet with 30-35 foot depths (inclusive of handicap accessible restroom areas), and 10-11 foot-high ceilings create a sufficient minimum area for shopkeeper uses to operate.
- j) Artisan loft space shall be developed with sufficient interior areas and ceiling heights to accommodate a variety of businesses,. Artisan loft widths of 15-20 feet with 18-25 depths (inclusive of handicap accessible rest-room areas), and 9-10 foot-high ceilings create a sufficient minimum area for artisan uses to operate, although higher ceilings may be appropriate in certain circumstances.



### 2) Guidelines:

- a) Higher density, pedestrian-oriented and mixed-use projects are encouraged that add to the economic viability of the downtown area.
- b) Porches, stoops, balconies and recessed entries in residential projects are encouraged. Design of these elements should retain a sense of privacy, but still allow interaction with the sidewalk while providing 'eyes on the street.'

# Downtown Specific Plan

## LAND USE DISTRICTS Historic Downtown

### 2. LOCATION

The Historic Downtown District encompasses approximately 170 acres generally located between Second and Third Avenues on the south, Valley Parkway on the north, Centre City Parkway on the west and Palomar Hospital on the east. The District includes over six blocks on both sides of Grand Avenue, which has been historically classified as Escondido's 'Retail Core Area'. Valley Parkway and Second Avenue provide convenient vehicular access to the Downtown Retail Core and to off-street parking lots, while Grand Avenue brings users to the center of activity and offers convenient on-street parking. A pedestrian trail connects the District to the Escondido Transit Center and citywide trail system, while pedestrian walkways and arcade pass-throughs and 'paseos' provide convenient pedestrian connections within Downtown.

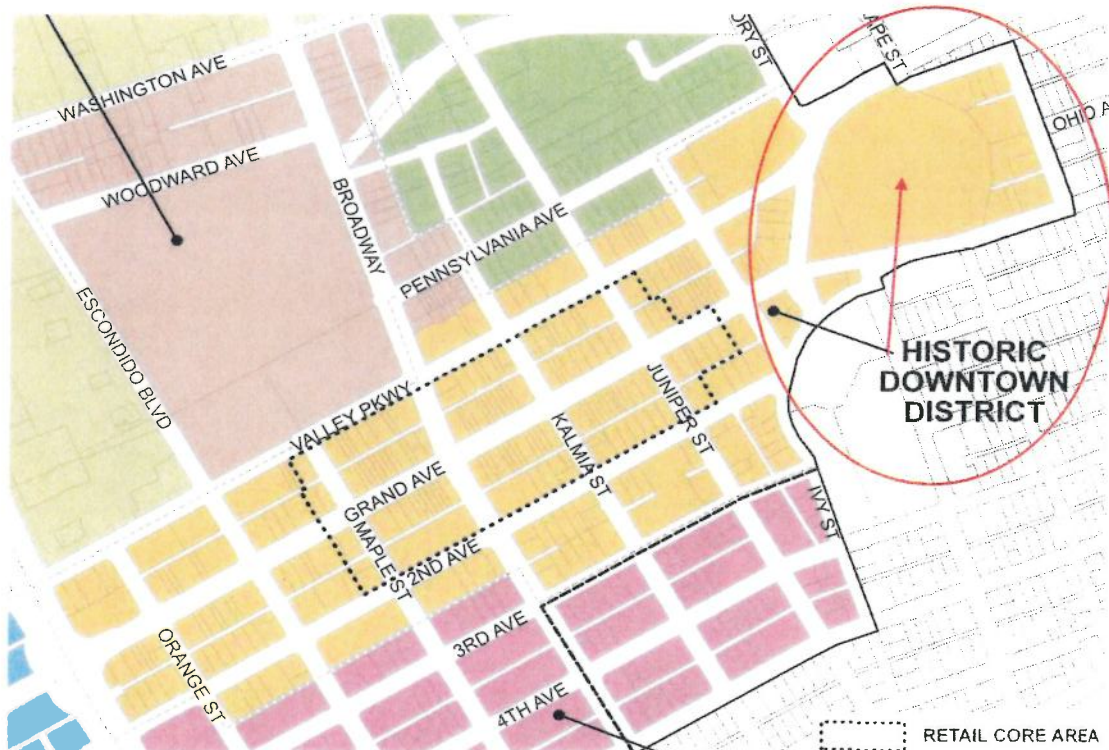


FIGURE V-2

Areas east and west of the Downtown Retail Core provide for expansion and development of offices and services. Businesses in these areas include a full range of professional and medical office uses, but might involve retail establishments based on market demand. Non-residential ground-floor uses are required throughout the Historic Downtown District. Mixed-use developments, with residential uses on upper floors, are encouraged for areas east of Maple Street. A larger concentration of financial and office development is located generally west of Maple Street. This area is intended for more intense non-residential commercial and office development to support a larger downtown workforce.



# Downtown Specific Plan

## LAND USE DISTRICTS *Historic Downtown*

### **b. Outside the Retail Core Area**

Mixed-use projects are permitted and encouraged for areas east of Ivy Street. Such mixed-uses include: retail and service commercial uses, restaurants, medical, administrative and professional office uses combined with residential uses on upper floors that include opportunities for higher densities.



## 4. HISTORIC DOWNTOWN DISTRICT DEVELOPMENT STANDARDS

### **a. Maximum Building Heights, Frontage, Setbacks, Lot Size, Coverage**

Refer to Figure III-5 (*Development Standards*).

### **b. Landscaping**

Landscaping shall conform to Article 62 of the Escondido Zoning Code or as determined appropriate through the Planned Development process.

### **c. Open Space: Mixed-Use Projects**

Unless separately established through the Planned Development application process, the minimum usable open space for the residential portion of a mixed-use project shall be 300 square feet per unit. Refer to Chapter III to determine what area(s) may qualify for calculating open space.